



# Weekly Management Report

## July 2, 2020

- 1. Synopsis**      Downtown Burbank Partnership (PBID) Meeting on  
June 11, 2020  
**Community Development Department**
  
  - 2. Memo**            The Process to Receive Input from Boards,  
Commissions, and Committees as it Relates to the  
Design Process  
**Community Development Department**
  
  - 3. Memo**            Amendments to Citywide Complete Our Streets  
Plan from City Council Public Hearing on June 16, 2020  
**Community Development Department**
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# MEMORANDUM



## COMMUNITY DEVELOPMENT



**DATE:** June 25, 2020

**TO:** Justin Hess, City Manager

**FROM:** Patrick Prescott, Community Development Director   
VIA: Simone McFarland, Asst. Community Development Director   
Mary Hamzoian, Economic Development Manager  
BY: Marissa Minor, Economic Development Analyst II

**SUBJECT:** Downtown Burbank Partnership (PBID) Meeting – June 11, 2020

- Staff made several announcements to the Board:
  - CDD offices will reopen to the public via appointment beginning June 15th. Most services will continue to be made available online and via e-mail.
  - Fabrication of the first Visit Burbank selfie-spot Batman statue has begun. Installation is expected in September 2020.
  - In an effort to help facilitate LA County's current physical distancing recommendations, the PBID has been working with the City to discuss potential changes to outdoor dining in Downtown Burbank. The first prospective change would use street parking along San Fernando Blvd. to create communal outdoor dining parklets providing additional seating for businesses in Downtown. The second potential program, would allow businesses to apply for expanded outdoor dining area, beyond what is currently permitted, onto sidewalks. Staff has received feedback from property owners in favor of this program and will provide an update at an upcoming meeting.
- Staff from Downtown Burbank's marketing agency, Anyone Collective reviewed the soon-to-be-launched Downtown Burbank website hosted at: [www.dtnbur.com](http://www.dtnbur.com)
- Staff provided an update on the City's Economic Recovery Plan, which was approved by City Council on May 19th. The Plan is intended to implement both short and long-term strategies to lessen the impacts of COVID-19 on our community including prevention of homelessness, encouragement of development and business recovery, and creation of financial stability for residents.
- Downtown Burbank events will be postponed or rescheduled based on orders or regulations from the LA County.

- Arts Fest: Postponed to December 5 – 6, 2020
  - Car Classic: Postponed to October 2020, confirmed date to be announced
  - Winter Wine Walk: No planned change, scheduled for November 14, 2020
  - The Rink in Downtown Burbank: No planned change, scheduled for December 10, 2020 through January 3, 2021
- Staff presented a holiday décor proposal from the Christmas Light Guy Co. to the Board for review. The company was selected via interviews by the PBID's Holiday Décor Subcommittee. Some holiday décor items proposed will need to go through Public Works for final approval, and for this reason, may be modified. The Board decided to pursue a 2-year contract with option to extend for a third year and directed staff to work on a draft agreement for final approval in July.



# MEMORANDUM




## COMMUNITY DEVELOPMENT



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**DATE:** June 22, 2020

**TO:** Justin Hess, City Manager

**FROM:** Patrick Prescott, Community Development Director   
VIA: Fred Ramirez, Assistant Community Development Director-Planning

**SUBJECT:** City Manager Tracking List Item #2049 – The Process to Receive Input From Boards, Commissions, and Committees as it Relates to the Design Process

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### PURPOSE

The purpose of this memo is to address City Council's inquiry regarding ways to incorporate input from boards, commissions, and committee members into the design/development review (DR) process. Staff recommends that the input of boards and commissions be channeled into the upcoming planning efforts related to the development of the Golden State Specific Plan, Burbank Center Plan Update, Greenhouse Gas Reduction Plan Update, Complete Streets Plan, and the 2021 Housing Element Update.

### DISCUSSION

With the exception of the Planning Board there is no formal means for City Boards, Commissions and Committees to provide input during the DR process. Even with the Planning Board it is not so much "input" that is provided as it is an evaluation of whether or not code required findings can be made to approve a project.

It is customary for Planning staff to engage boards, commissions and committees on long range planning projects such as updates to General Plan or a specific plan. More recently, it is common for Planning staff to conduct one on one stakeholder interviews with some members of certain boards and commissions which was done for the Complete Streets Master Plan.

### Early Community Participation in Policy Formulation is Key

For this reason, active community participation including the input of boards, commissions and committee members in long range planning documents can improve future development's overall design through the use of local knowledge which can increase future project acceptability, produce a more equitable distribution of community benefits, and help ensure environmentally sustainable development.

Staff acknowledges the benefits of community participation including those of the City's boards, commissions, and committee members and recommends channeling their valuable insight as stakeholder input in upcoming planning efforts. This stakeholder input will help formulate objective design and development standards for new development, such as housing and mixed use projects within the Golden State Specific Plan and Burbank Center Plan, as well as the other aforementioned efforts to update the City's Climate Action Plan and Housing and Safety elements. These long-range planning efforts will continue to raise the bar for new development in Burbank.

## **ANALYSIS**

### Purpose of the City's Design/Development Review

Burbank, like many jurisdictions, relies on the discretionary review processes when considering most development projects. Per the Burbank Municipal Code, DR is intended to: preserve stability of existing residential neighborhoods; provide suitable living environments; promote quality of design in commercial and industrial development as well as multi-family residential development; promote orderly, attractive and harmonious development; facilitate a balance of housing types and values; prevent deterioration of local air quality; and to ensure that traffic demands do not exceed the capacity of streets. DR is also intended to coordinate growth and to control the building of structures so that compatibility is maintained between new development and existing residential neighborhoods. Furthermore, DR is intended to expedite and streamline the building permit process. By gathering information at the earliest possible stage and exchanging that information between the property owner/developer and the City staff, the performance standards of the City's zoning code and specific plans can be maintained and implemented at the earliest stages of the development process.

The current DR process focuses on a series of conversations between staff members in the Planning division, between staff members in different departments, between City staff and the development team, and eventually culminates in a public information/community meeting at which time the development team presents the latest version of the project to the community for

public input. As a result of this input, City staff continues its discussions with the development team to address community concerns to the extent feasible while also ensuring that the final design is in compliance with applicable City zoning and building codes. This process concludes with the administrative approval by the Community Development Director, or in the case of projects that require further discretionary approval, review by the Planning Board and/or the City Council as is the case for a planned development project.

#### Communication of Community Expectations

The communication of community expectations up front to developers is key in ensuring that a project facilitates responsible development that helps build neighborhoods by implementing project design features at the earliest stages of the development review process. The most effective way to communicate community expectations and therefore facilitate greater transparency in the development review process is to incorporate objective design and development standards that reflect the community character and vision through the City's General Plan and Zoning Code and by extension the City's specific plans, which include the proposed Golden State Specific Plan and Burbank Center Plan Update, which must be followed by new development that is seeking City approval.

It is in this area that input from boards, commissions and committee members should be focused as part of stakeholder input in developing objective design and development standards. The collective effort of community stakeholders, boards and commissions, and staff to establish these objective criteria will be key as the City seeks to continue protecting community character and single family neighborhoods while still facilitating new projects that produce new housing units and quality commercial development. It is staff's assessment that this approach will help improve communication of community expectations to the developers and not result in projects that are delayed or attempted to be redesigned at the last minute, which could result in a project that never gets built and "dies by a thousand cuts".

#### CONCLUSION

Community participation including the input of boards, commissions and committee members can help facilitate the development of objective design and development standards that can improve project design and help build not just buildings, but neighborhoods. Potential state legislation which provides for shortened City review periods and the use of objective development standards to review larger development projects make it imperative that public input be sought in the development of specific plans and zoning code amendments. This early



stakeholder input, before projects are even submitted, will ensure that the developer and City staff reviewing the project are on the same page as to what is expected by the community regarding future development proposals. Included as part of this effort will be ongoing communication and feedback from the various boards, commissions and committees. This ongoing dialogue will ensure that long range planning policies are not only being met, but when necessary, updated to address changes in law and/or community needs. Therefore, staff recommends that community participation that includes the ongoing input of boards, commissions and committee members be channeled into the upcoming planning efforts related to the Golden State Specific Plan, Burbank Center Plan Update, Greenhouse Gas Reduction Plan Update/Climate Action Plan, Complete Streets Plan, and the 2021 Housing Element and Safety Element Update.



# MEMORANDUM



**COMMUNITY  
DEVELOPMENT**



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**DATE:** June 22, 2020

**TO:** Justin Hess, City Manager

**FROM:** Patrick Prescott, Community Development Director 

**SUBJECT:** City Council Resolution Amendments to Citywide Complete Our Streets Plan from City Council Public Hearing on June 16, 2020

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At the City Council Meeting on June 16, 2020, Council requested amendments to the Resolution to adopt the Citywide Complete Our Streets Plan (Plan). Council voted to make the following amendments:

1. Add language summarizing the history of the Rancho District in Chapter 2 Existing Conditions Analysis in the Plan;
2. Add language to have the option to incorporate Green Streets elements to new public parking lots;
3. Add language to page 172 for the Mariposa St. Bridle Path Project about the importance of adding the missing connection in the Rancho community to Griffith Park and neighboring disadvantaged areas in the City; and
4. Add language to expand the possibility of future public outdoor space.

The amendments have been incorporated into the Resolution and in the Citywide Complete Our Streets Plan document. Please refer to Attachments A and B for the amendments to the Plan.

## **ATTACHMENTS**

Attachment 1: Amended Resolution for the Citywide Complete Streets Plan

Attachment 2: Amended Pages of Citywide Complete Streets Plan

# **ATTACHMENT 1**

RESOLUTION NO. \_\_\_\_\_

A RESOLUTION OF THE COUNCIL OF THE CITY OF BURBANK ADOPTING THE BURBANK CITYWIDE COMPLETE STREETS PLAN AND DECLARING CATEGORICAL EXEMPTION UNDER CEQA.

THE COUNCIL OF THE CITY OF BURBANK FINDS:

A. On April 28, 2017, the California Legislature passed and Governor Edmond G. Brown Jr. signed into law Senate Bill (SB) 1 – The Road Repair and Accountability Act of 2017, a transportation funding bill that will provide a reliable source of funds to maintain and integrate the State’s multi-modal transportation system;

B. The SB 1 grant funds are intended to support and implement the Regional Transportation Plan (RTP) Sustainable Communities Strategies (SCS) and to ultimately achieve the State’s greenhouse gas (GHG) reduction target of 40 and 80 percent below 1990 levels by 2030 and 2050, respectively;

C. On February 13, 2018, the City of Burbank accepted the Caltrans Sustainable Transportation Planning Grant – Road Maintenance & Rehabilitation Account (RMRA) – to create a Citywide Complete Streets Plan in the amount of \$519,228.00. A local match of \$67,272.00 in City funds was provided for a total project amount of \$586,228.00.

D. The City of Burbank Complete Streets Plan (“Citywide Complete Streets Plan”), attached to this Resolution as Exhibit A, fulfills the following Burbank2035 General Plan Mobility Element Goal 3, Complete Streets, which states that Burbank’s complete streets will meet all mobility needs and improve community health. Goal 3 also has the following policies:

1. Policy 3.1: Use multi-modal transportation standards to assess the performance of the City street system.
2. Policy 3.2: Complete City streets by providing facilities for all transportation modes.
3. Policy 3.3: Provide attractive, safe street designs that improve transit, bicycle, pedestrian, and equestrian connections between homes and other destinations.

4. Policy 3.4: All street improvements should be implemented within the existing right-of-way. Consider street widening and right-of-way acquisition as methods of last resort.
5. Policy 3.5: Design street improvements so they preserve opportunities to maintain or expand bicycle, pedestrian, and transit systems.

E. The Citywide Complete Streets Plan is statutorily exempt under California Environmental Quality Act (CEQA) Title 14, Article 18, Section 15262 as a planning study as it does not commit the City to implement any of the identified improvements that will be included in or approved by the Plan.

**THE COUNCIL OF THE CITY OF BURBANK RESOLVES:**

1. The Citywide Complete Streets Plan has been processed in accordance with the applicable provisions of the California Environmental Quality Act (CEQA).
2. The Citywide Complete Streets Plan is hereby adopted to guide future improvements to enhance safety for all modes, users, ages, abilities, and disabilities of the transportation system.

PASSED and ADOPTED this \_\_\_\_ day of \_\_\_\_\_, 2020.

\_\_\_\_\_  
Sharon Springer  
Mayor of the City of Burbank

Attest:

\_\_\_\_\_  
Zizette Mullins, MMC, City Clerk

Approved as to Form  
Office of the City Attorney

By: \_\_\_\_\_  
Iain MacMillan  
Assistant City Attorney

STATE OF CALIFORNIA            )  
COUNTY OF LOS ANGELES    ) ss.  
CITY OF BURBANK                )

I, Zizette Mullins, MMC, City Clerk of the City of Burbank, do hereby certify that the foregoing Resolution was duly and regularly passed and adopted by the Council of the City of Burbank at its regular meeting held on the \_\_\_\_ day of \_\_\_\_\_, 2020, by the following vote:

AYES:

NOES:

ABSENT:

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Zizette Mullins, MMC, City Clerk



# ATTACHMENT 2

## 2A. GEOGRAPHICAL ALIGNMENT

In the late 19th century, Dr. David Burbank, the eponymous sheep farmer, owned the farmland that ultimately became the City of Burbank. He sold a portion of his holdings to the Southern Pacific Railroad (SPRR) and by 1874, a rail line from Los Angeles to San Fernando was completed and a waystation was established in what would become Downtown Burbank. In 1886, Dr. Burbank sold his remaining property to land speculators, who formed the Providencia Land, Water & Development Company. They divided the land, sold lots and farms, and named their small town, Burbank.

The young settlement's streets first aligned themselves along the rail corridor. This was the origin of the Downtown Burbank Grid, which has left a lasting and immediately recognizable imprint in the City's urban core.



City, Aug. 1887 (Source: LA Public Library)



City, Aug. 1907 (Source: LA Public Library)

The Magnolia Park Grid was a result of the Chatsworth Branch of the Southern Pacific Railroad network. It split to the west in 1895, which established the alignment of streets in Magnolia Park. Today, the Chatsworth Branch is better known as the Chandler Bikeway, replacing the train with a new mode of travel in Burbank.



Magnolia, Aug. 1905 (Source: LA Public Library)

In the northwest, where the City interfaces with the San Fernando Valley, the street grid reverted to the Valley Grid displaying the cardinal-direction orientation seen elsewhere in the region.

The Rancho District grid grew up around the presence of Griffith Park and the Los Angeles River. It emerged as a horse-keeping district as the movie studios began filming 100's of westerns in the early/mid-20th century. In 1938, actor/singer Gene Autry made a successful request of the City of Burbank to "lend every effort to see that the privileges of Griffith Park are not denied the [residents] of Burbank and those who patronize the local stables, by... procuring a permanent crossing over the river at or near Mariposa Street" (Burbank City Council minutes, August 23, 1938). An equestrian bridge was built into Burbank's grid, assuring a unique and thriving horse-keeping district at its southern boundary

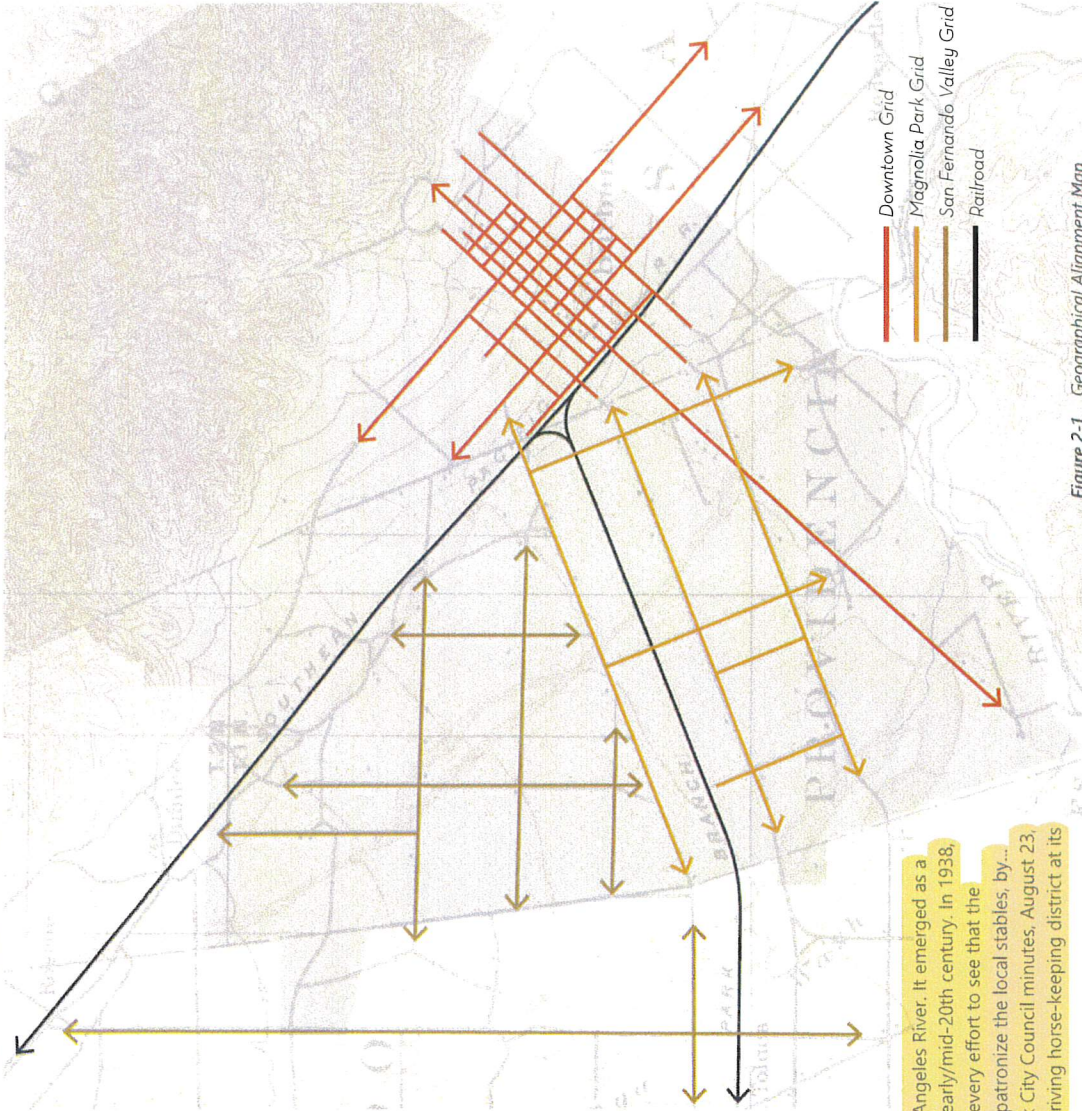


Figure 2-1. Geographical Alignment Map (Source: USGS, Los Angeles Area Map, 1894).



- Trees should NOT be located with 20 ft. of an intersection so as not to impede the ability of motorists to safely see pedestrians, bicyclists, and other vehicles in the roadway.

- Green streets elements should be considered when building new public parking lots.

**PLANTERS:**

Planters are typically above-grade or at-grade structures with vertical walled sides or edges. Depending on their system design, planters may either have open bottoms to infiltrate (or recharge) stormwater into the ground or closed bottoms to detain stormwater for a temporary time. Planters may consist of rock, gravel, soil, and/or vegetation, inclusive of trees, as appropriate for collecting, cleaning, storing, infiltration, and/or discharge of stormwater and stormwater pollutants. Refer to the Burbank Green Streets Manual for more information on the various types of green infrastructure planters, such as

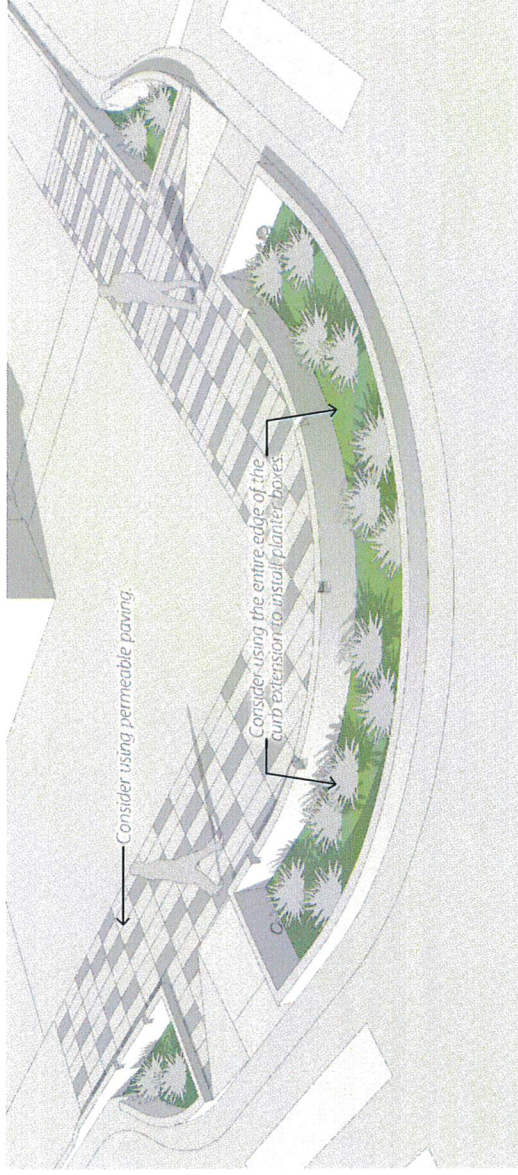


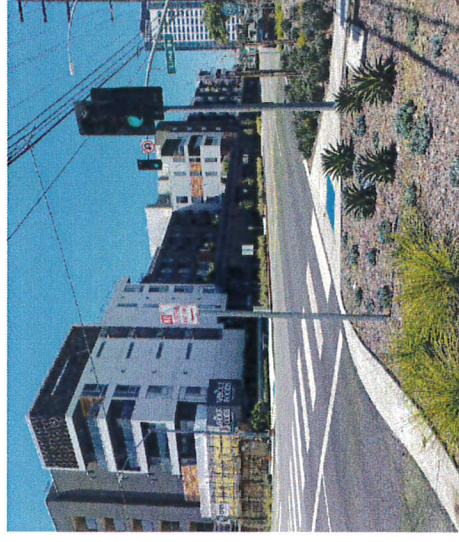
Figure 9-4. Intersection Corner Curb Extension Planters at a Low-Speed and Volume Street.



Curb Extension with Bioswale on Lake St. near BWP Campus.



Curb Extension with Bioswale on Magnolia Blvd. at San Fernando Blvd.



Curb Extension with Landscaping on Alameda Ave at Lima St.



### 3 SKewed INTERSECTIONS

Skewed intersections are those where streets intersect at an angle other than 90 degrees. When these intersections are reconfigured so that streets intersect as close to 90 degrees as possible, space may be recaptured for purposes of green infrastructure. Consider the following at skewed intersection reconfigurations:

#### SIDEWALKS/PARKWAYS:

- In general, reconfigurations of intersections should be used as opportunities to complete or extend adjacent sidewalks/parkways such that pedestrian pathways are continuous.
- Along the sidewalks/parkways of streets at reconfigured skewed intersections, see [Chapter 9D-1 Sidewalks/Parkways](#) on page 126.

#### POCKET PARKS:

- Unlike a neighborhood or city park, pocket parks are small outdoor open spaces, usually no more than a quarter-acre in size, that are created out of reclaimed areas. Pocket parks can be programmed into a variety of uses, e.g., active uses, such as plazas or play areas for children, or passive areas, such as gardens.

- Consider adding public outdoor spaces to increase opportunities for passive recreation, seating, and outdoor dining.

#### DEMONSTRATION GARDENS:

- Demonstration gardens provide outdoor spaces for landscape education. Usually adopted and maintained by local community organizations, demonstration gardens allow for hands-on experience and community involvement on a variety of topics, such as:
  - Bird, Butterfly, or Pollinator Gardens
  - Native Plant Or Drought-Tolerant Gardens
  - Rain Gardens
  - Urban Farming
- Demonstration gardens will need to be maintained with proper pruning, mulching, and plant replacement as necessary. Opportunities may be found for local community groups and/or community members to adopt the gardens to help maintain and spread education/awareness of the importance of these public spaces.

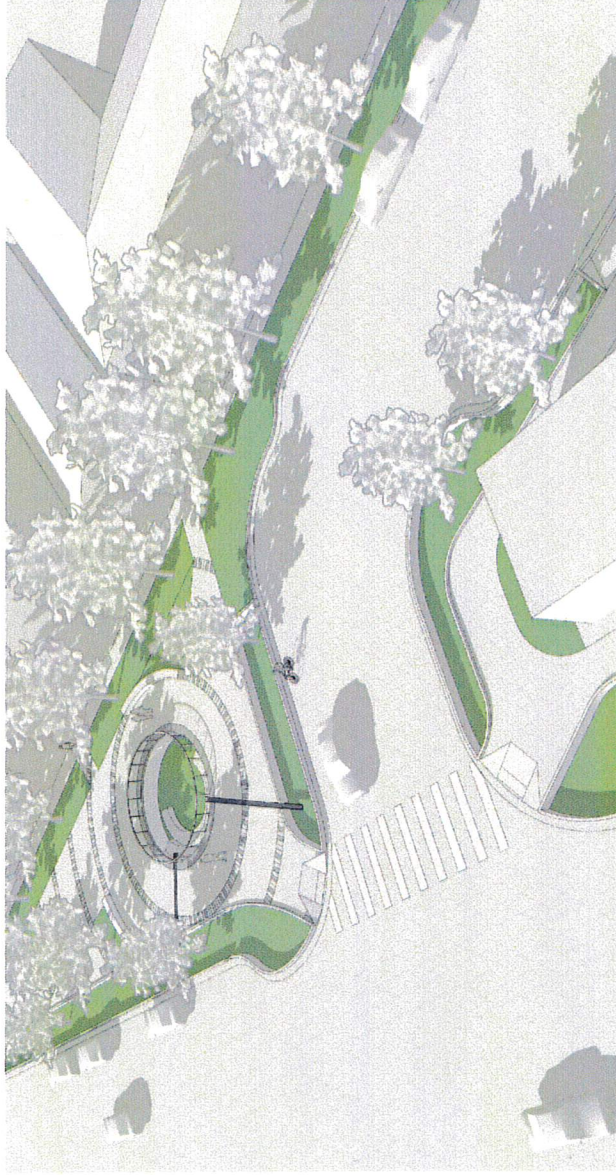
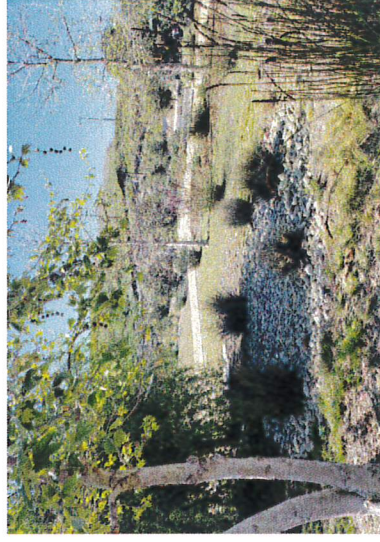


Figure 9-10. Park or Garden at a Reconfigured Skewed Intersection.



Airport Ave. Garden in Santa Monica, CA (Source: LRM)



Stoneview Nature Center in Culver City, CA (Source: LRM)



# LONG-TERM PRIORITY PROJECT

## MARIPOSA ST. BRIDLE PATH PROJECT

### PROJECT LOCATION:

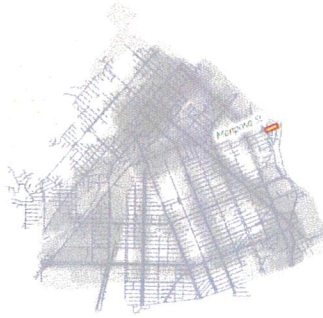
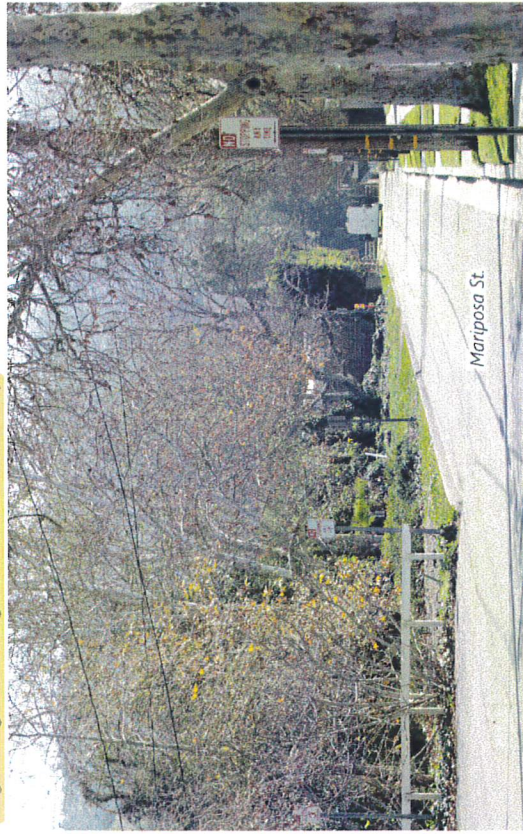
Mariposa St. between Riverside Dr. and Valleyheart Dr.

### PROJECT DESCRIPTION:

- Request for future dedication from adjacent private property along eastern side of Mariposa St. and remove about eight on-street parking spaces to construct a 12 ft. wide equestrian path.
- Construct a 4 ft. high fence to separate equestrians from other street users.

### PURPOSE:

- Project would improve equestrian access to the only equestrian bridge that connects Burbank to Griffith Park.
- Improve equestrian, pedestrian, and motorist safety.
- Provide missing connection from Rancho District to Griffith Park and neighboring disadvantaged communities in the City.



### Priority Eligibility:

- Priority networks: **Pedestrian, Equestrian**
- Within focus areas: **No**

### Average Annual Collisions within a quarter mile (June 2013 - June 2018):

- Total collisions: **2.8 per year** (citywide average: 1,302.4 per year)
- Pedestrian collisions: **0 per year** (citywide average: 61.4 per year)
- Bicyclist collisions: **0 per year** (citywide average: 53.2 per year)
- Motorist-only collisions: **2.6 per year** (citywide average: 1,175.2 per year)
- Total collision severity: **0 of 14 collisions (0%)** resulted in KSI incidents
- Pedestrian collision severity: **0 of 0 collisions (0%)** resulted in KSI incidents
- Bicyclist collision severity: **0 of 0 collisions (0%)** resulted in KSI incidents
- Motorist-only collision severity: **0 of 13 collisions (0%)** resulted in KSI incidents

### Socio-Economic Indicators:

- CallEnviroScreen 3.0 Percentile Scores: **55-60%**

**Planning-Level Rough-Order-of-Magnitude (ROM) Cost Estimate: \$950K**

**Annual Operations and Maintenance (O&M) Cost Estimate: \$25K**

