

# Rancho Providencia Neighborhood Protection Plan Update

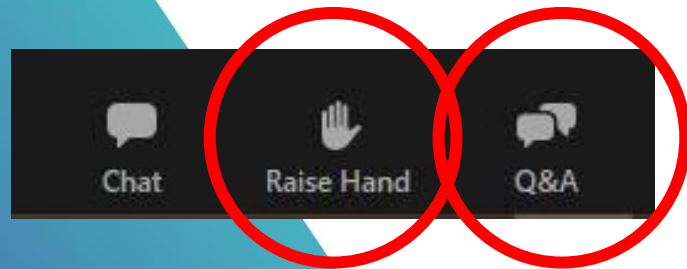
Chris Buonomo  
Assistant Transportation Planner

4.7.2022



**CITY OF  
BURBANK**

# WORKSHOP DYNAMICS



- 1.5-hour maximum
- Public Comment - Raise Hand - 1 minute max
- Q&A always enabled on side panel
- Questions may not be answered during webinar, but will be posted on website
- Contact Project Leads



# THE TEAM

David Kriske - Asst. Community Development Director, Transportation

Chris Buonomo - Asst. Transportation Planner, Community Development Dept

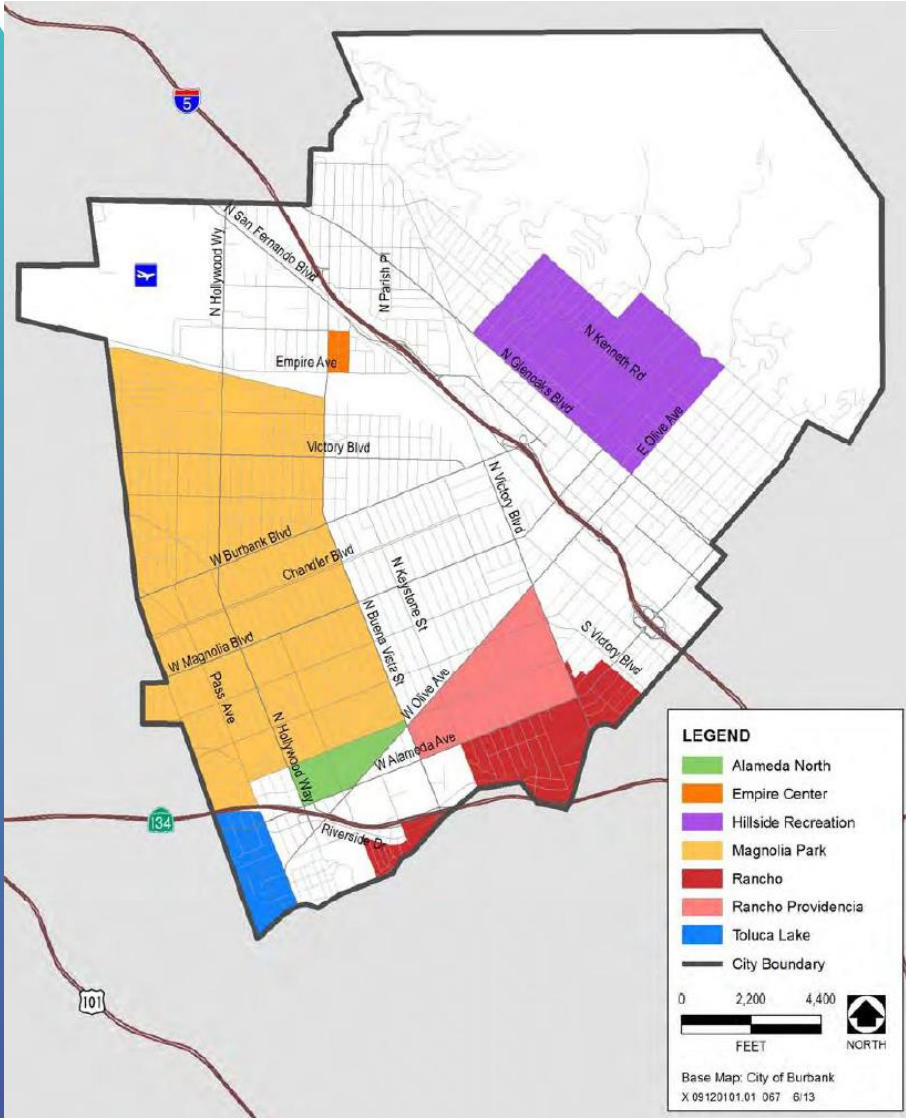
Joanne Itagaki - Project Manager, Willdan Engineering



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# NPP OVERVIEW





# RANCHO PROVIDENCIA NPP HISTORY



Late 90's: Regional traffic generated by studios and commercial properties in Media District

1998: City Council adopted NPP to address:

- Cut-through traffic
- Speeding
- Spillover of commercial parking

2001: Permanent measures installed





Olive Ave



Olive Ave



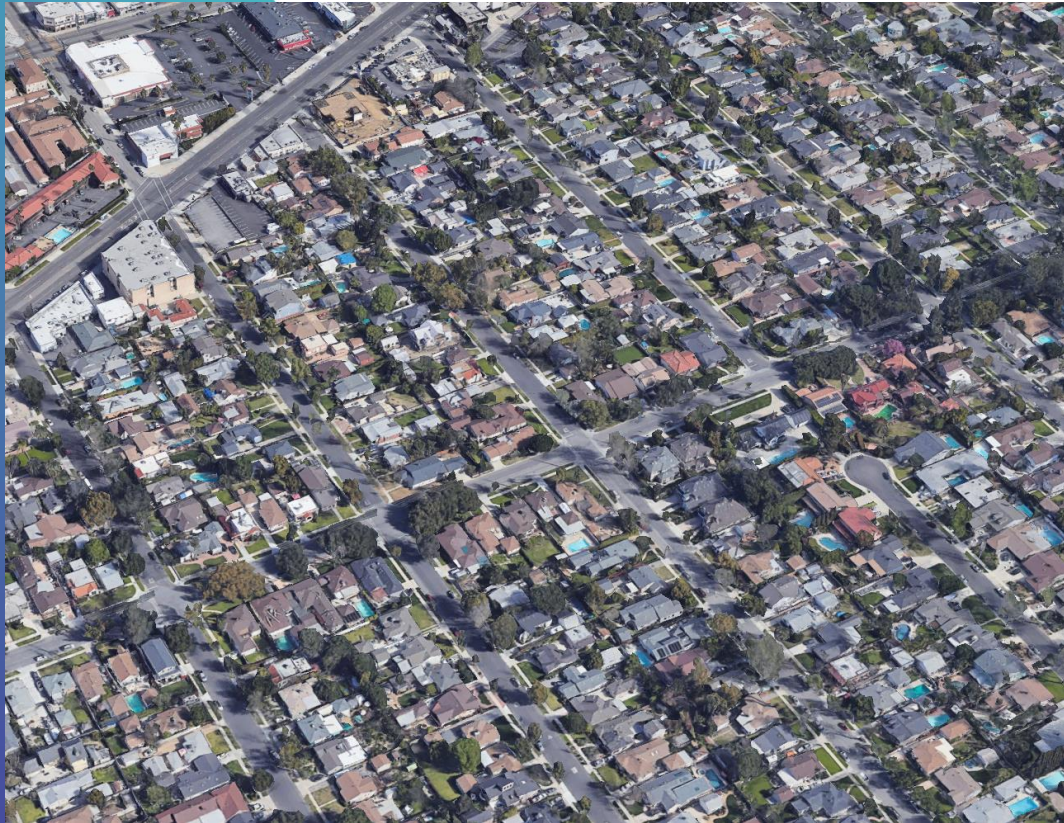
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# WHAT WE'VE HEARD



## Recent Past

Need to alleviate:

- Cut-through traffic
- Neighborhood parking
- Speeding

## Present

- Raisin' Cane's
- Metro BRT
- Olive/Sparks/Verdugo intersection

## Future

Does the 1998 NPP still serve the neighborhood effectively?

# PHASED APPROACH

Phase 1: Short-term temporary measures

- Immediate need to control spill-over and cut-through traffic around drive-thru
- Could include turn restrictions and traffic control

Phase 2: Long-term NPP update

- Broader analysis of entire NPP
- Assessment of Phase 1 measures
- CEQA documentation

# STATUS

Contracted with on-call consultant who collected data





# TIMELINE

## Two-Phase Approach:

1. Short-term temporary measures
2. Long-term NPP update



Phase 1: Temporary Measures to Address Restaurant Traffic	
Data Collection and Input	Jan-Apr '22
Evaluate Phase 1 Temporary Measures	Apr '22
Temporary Measures Signing and Striping	May '22
Phase 2: Rancho Providencia NPP Update	
1998 Evaluation Plan	Mid-Apr '22
Identify Additional Improvements	June '22
Draft Plan (Final Plan)	Oct '22
Final Environmental Documentation for Adoption	Oct '22

← Current Step

● =Community Meeting (dates TBD)

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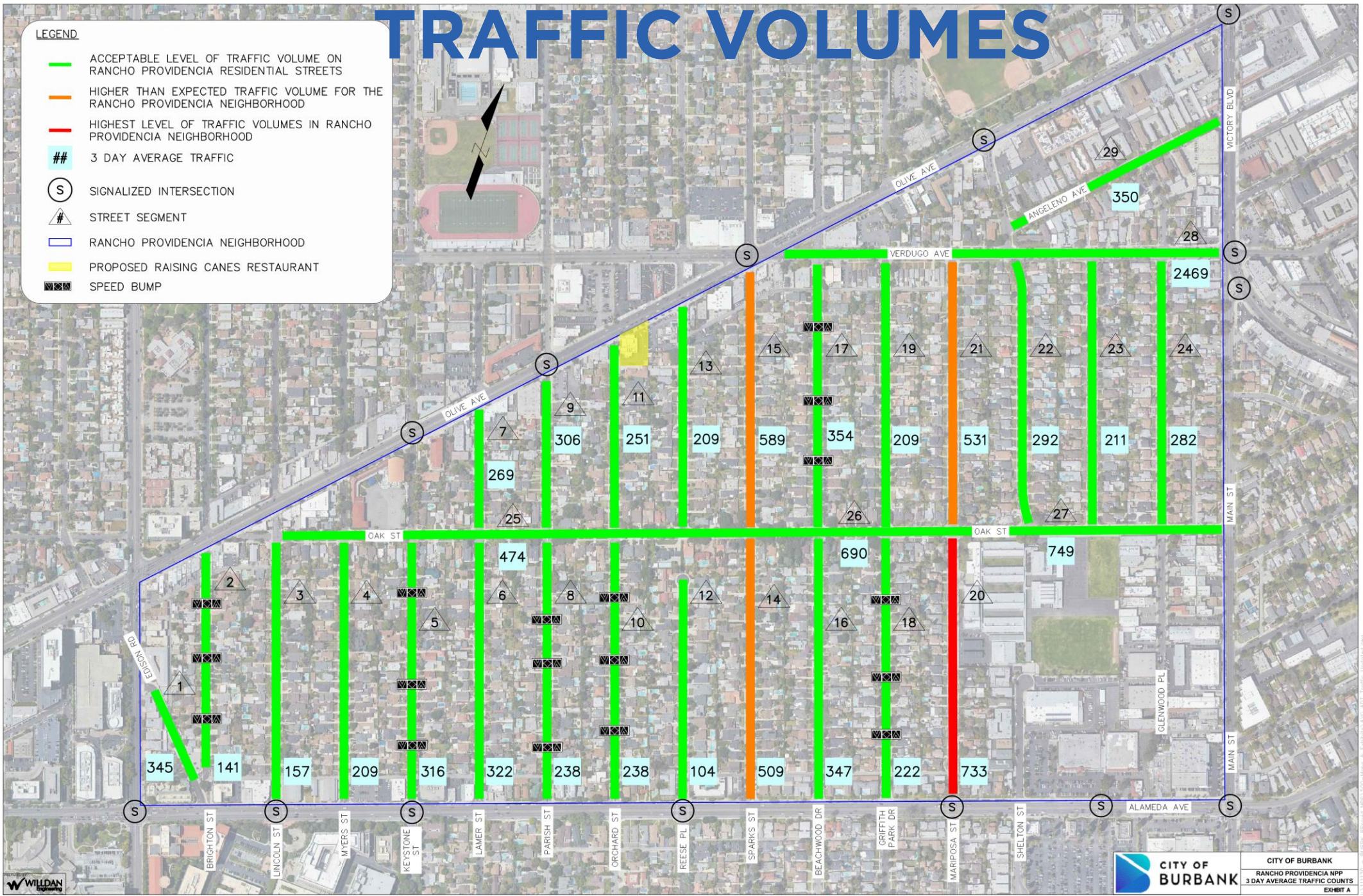
# **DATA PRESENTATION**



# TRAFFIC VOLUMES

**LEGEND**

- ACCEPTABLE LEVEL OF TRAFFIC VOLUME ON RANCHO PROVIDENCIA RESIDENTIAL STREETS
- HIGHER THAN EXPECTED TRAFFIC VOLUME FOR THE RANCHO PROVIDENCIA NEIGHBORHOOD
- HIGHEST LEVEL OF TRAFFIC VOLUMES IN RANCHO PROVIDENCIA NEIGHBORHOOD
- ## 3 DAY AVERAGE TRAFFIC
- (S) SIGNALIZED INTERSECTION
- ▲ STREET SEGMENT
- ▭ RANCHO PROVIDENCIA NEIGHBORHOOD
- ▭ PROPOSED RAISING CANES RESTAURANT
- ▭ SPEED BUMP

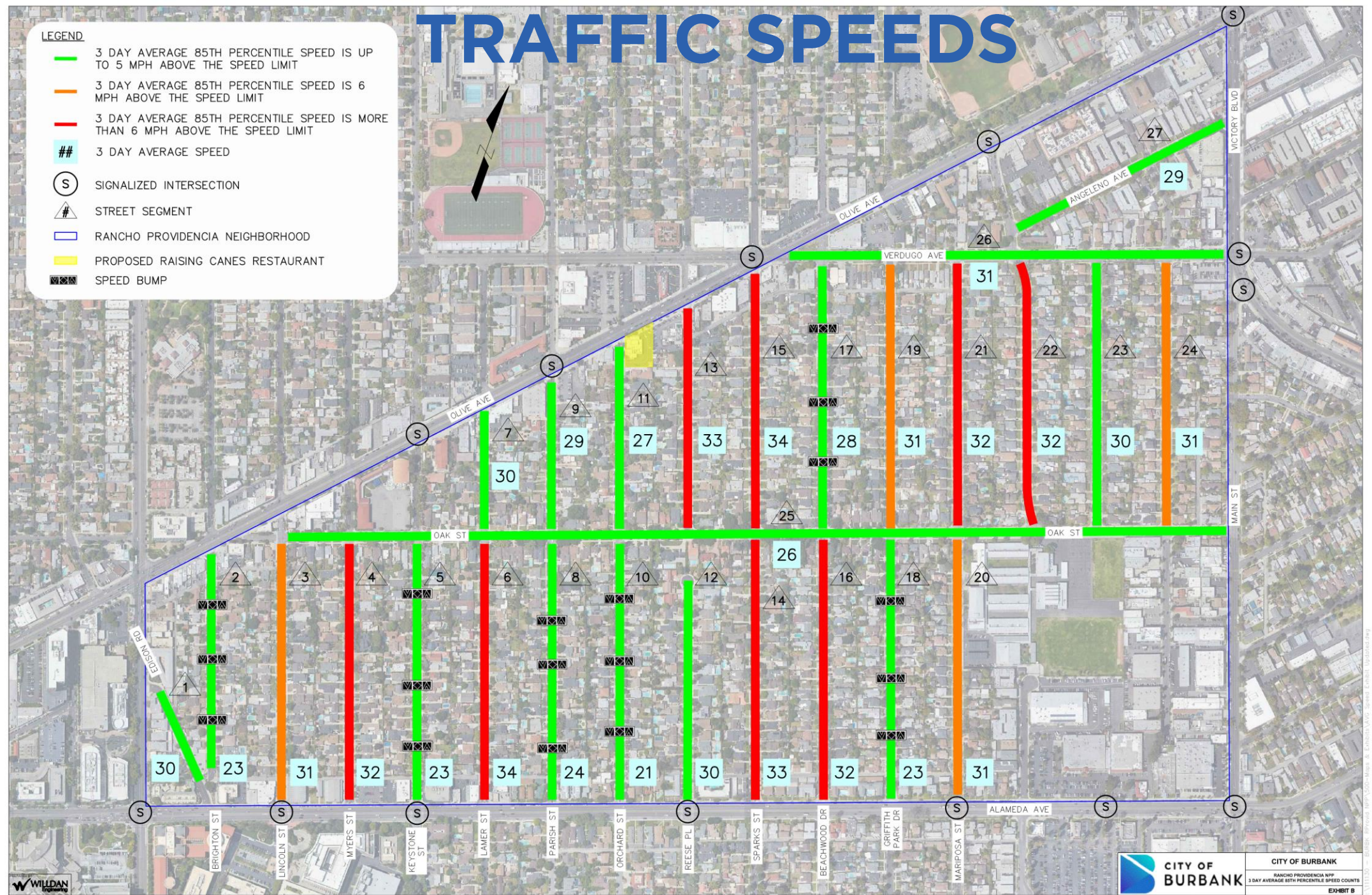




# TRAFFIC SPEEDS

**LEGEND**

- 3 DAY AVERAGE 85TH PERCENTILE SPEED IS UP TO 5 MPH ABOVE THE SPEED LIMIT
- 3 DAY AVERAGE 85TH PERCENTILE SPEED IS 6 MPH ABOVE THE SPEED LIMIT
- 3 DAY AVERAGE 85TH PERCENTILE SPEED IS MORE THAN 6 MPH ABOVE THE SPEED LIMIT
- ## 3 DAY AVERAGE SPEED
- (S) SIGNALIZED INTERSECTION
- # STREET SEGMENT
- ▭ RANCHO PROVIDENCIA NEIGHBORHOOD
- ▭ PROPOSED RAISING CANES RESTAURANT
- ▭ SPEED BUMP





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# QUESTIONS AND COMMENTS

Neighborhood observations, experience.

What has worked? What do you like?

What are your concerns?

What roadway conditions do you wish to see?

# CONTACT

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