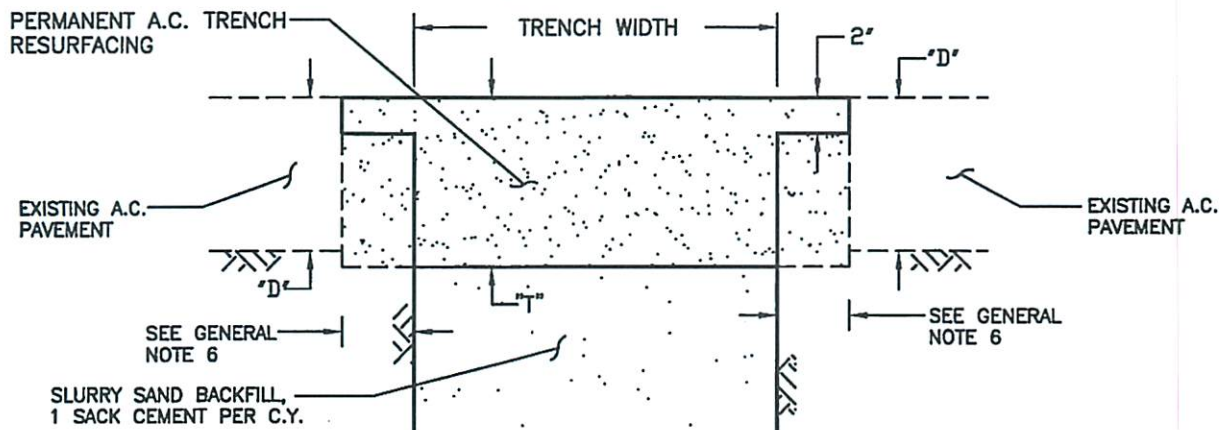
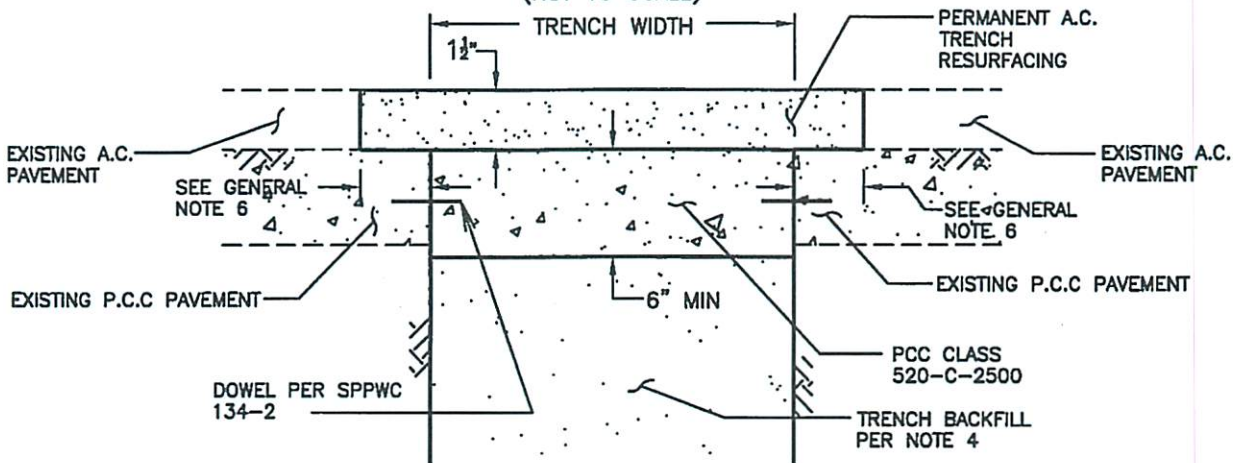


CASE I: ASPHALT CONCRETE PAVEMENT (NO BASE)
(NOT TO SCALE)



CASE II: ASPHALT CONCRETE PAVEMENT
(POOR SOIL SUBGRADE OR TRENCH WIDTH LESS THAN 2')
(NOT TO SCALE)



CASE III: ASPHALT CONCRETE PAVEMENT
(CONCRETE BASE)
(NOT TO SCALE)

CITY OF BURBANK - PUBLIC WORKS DEPARTMENT

REV	BY	DATE
1	RS	12/16
2	RS	10/17
3	RS/EF	03/01/19

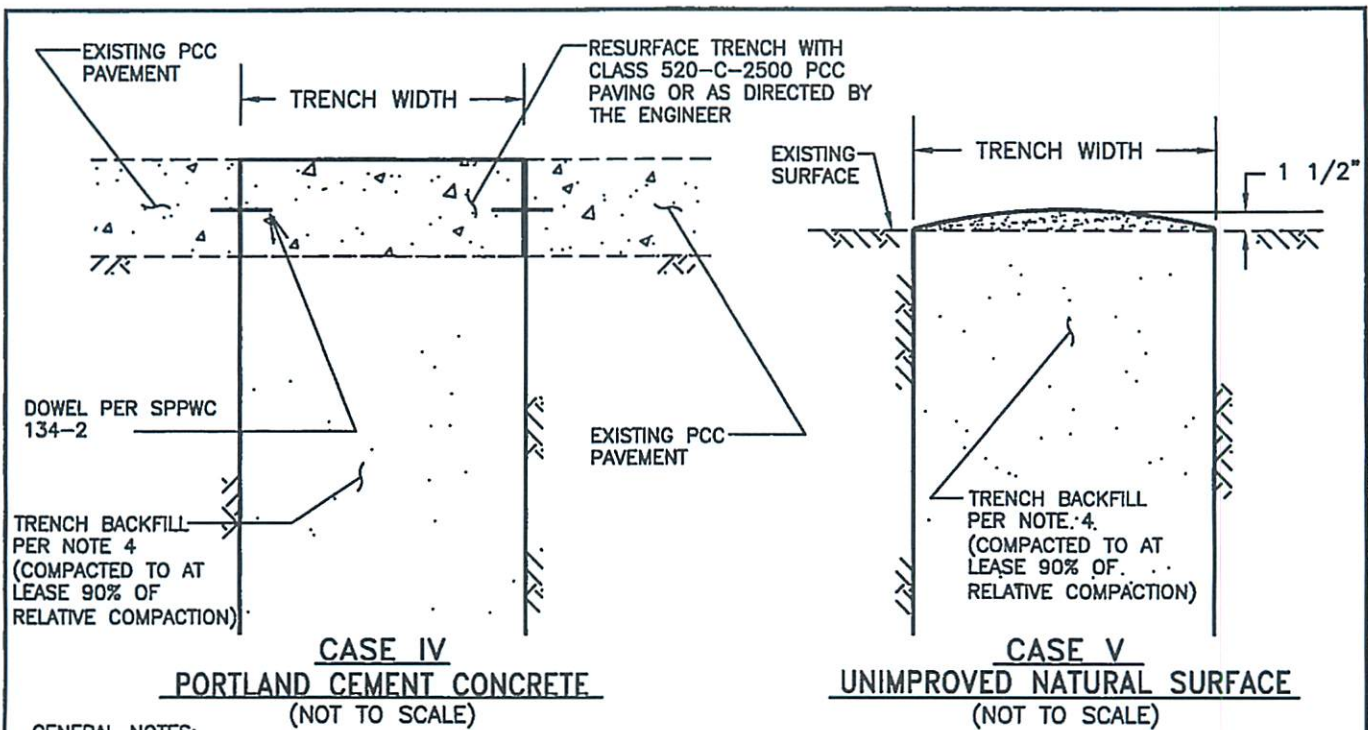
TRENCH RESURFACING/STREET RESTORATION

APPROVED BY

David J. [Signature]
CITY ENGINEER

3/13/19
DATE

STANDARD PLAN
BS-110-3
SHEET 1 OF 3



GENERAL NOTES:

1. ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE LATEST ADOPTED STANDARD SPECIFICATION FOR PUBLIC WORK CONSTRUCTION (GREEN BOOK) AND PER REFERENCED STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION.
2. ALL PAVEMENT REMOVALS SHALL BE MADE TO CLEAR STRAIGHT LINES SAW CUT TO A MINIMUM OF 1-1/2 INCHES DEEP. IF NEW CUT LINE IS LESS THAN THREE (3) FEET FROM PREVIOUS CUT LINES, OR LESS THAN THREE (3) FEET FROM CONCRETE EXPANSION JOINTS AND EDGES. THE EXISTING PAVEMENT SHALL BE REMOVED TO SAID CUT LINES, JOINTS AND EDGES OR AS DIRECTED BY THE ENGINEER.
3. DURING TRENCHING EXCAVATIONS AND SUBGRADE PREPARATION, THE CONTRACTOR SHALL TAKE ALL NECESSARY STEPS TO INSURE THE PROTECTION OF ALL IMPROVEMENTS, WHETHER PUBLIC OR PRIVATE, INCLUDING UTILITIES AND THEIR SERVICES, FROM ANY DAMAGE THAT COULD OCCUR DUE TO HIS OPERATION.
4. BACKFILL AND DENSIFICATION SHALL BE DONE IN CONFORMANCE WITH SECTION 306 OF THE STANDARDS SPECIFICATIONS, EXCEPT AS FOLLOWS:
 A) TRENCH BACKFILL SHALL BE DENSIFIED TO A MINIMUM OF 90% RELATIVE COMPACTION.
 B) WHEN PAVEMENT IS TO BE PLACED DIRECTLY ON SUBGRADE MATERIAL, THE TOP 6 INCHES OF SUBGRADE MATERIAL SHALL BE COMPACTED TO A RELATIVE COMPACTION OF 95%.
 * MUST PROVIDE CERTIFIED COMPACTION FOR A) AND B) OR MAY SUBSTITUTE WITH SLURRY SAND BACKFILL, 1 SACK CEMENT PER CUBIC YARD.
5. TEMPORARY TRENCH RESURFACING SHALL BE PLACED AT THE CONTRACTOR'S EXPENSE, UNLESS OTHERWISE SPECIFIED BY THE ENGINEER. IT SHALL BE PLACED LEVEL WITH THE EXISTING PAVEMENT ON COMPACTED TRENCH BACKFILL AND SHALL BE A MINIMUM OF 2 INCHES.
6. T-CUT FOR PAVEMENT WITH PCI POOR OR BELOW: TRENCH EDGES TO BE CUT A MINIMUM OF 6 INCHES WIDER THAN TRENCH FOR TWO (2) FEET WIDE OR LESS, AND TWELVE (12) INCHES WIDE FOR TRENCHES OVER TWO (2) FEET WIDE.
7. EXISTING PAVEMENT SHALL BE CUT AND REMOVED IN SUCH A MANNER SO AS NOT TO TEAR, BULGE OR DISPLACE ADJACENT PAVEMENT, EDGES SHALL BE CLEAN AND VERTICAL. ALL CUTS SHALL BE PARALLEL OR PERPENDICULAR TO STREET CENTERLINE, WHEN PRACTICAL.
8. PERMANENT PAVEMENT RESURFACING SHALL BE DONE TWO (2) WEEKS AFTER BACKFILL OF TRENCHES HAS BEEN COMPLETED, AND ONLY AFTER SETTLEMENT HAS TAKEN PLACE AND THE FILL SURFACE HAS SUFFICIENTLY DRIED. ALL CUTS SHALL BE CLEAN AND STRAIGHT. THE CONTACT SURFACES OF EXISTING PAVEMENT, MANHOLE FRAMES AND SHAFTS AND CONCRETE SURFACES SHALL BE GIVEN A TACK COAT PER 302-5.4 BEFORE PERMANENT ASPHALT TRENCH RESURFACING IS PLACED.
9. CASE I AND II PERMANENT TRENCH RESURFACING SHALL CONSIST OF AN ASPHALT CONCRETE SURFACE COURSE OF TYPE C2 PG 64-10 OVER AN AN ASPHALT CONCRETE BASE COURSE OF TYPE B PG 64-10. WHERE THE ORIGINAL ASPHALT PAVEMENT THICKNESS "D" IS SIX INCHES OR LESS, THE REPLACEMENT THICKNESS "T" SHALL BE A MINIMUM OF ONE INCH GREATER THAN "D" UP TO A MAXIMUM OF SIX INCHES, WHERE THE ORIGINAL ASPHALT THICKNESS IS SIX INCHES OR MORE, THE REPLACEMENT THICKNESS SHALL BE TO THE SAME THICKNESS OR AS DIRECTED BY THE ENGINEER.

CITY OF BURBANK - PUBLIC WORKS DEPARTMENT

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1	RS	12/16		
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APPROVED BY:			<i>Daryl J. ...</i> CITY ENGINEER	3/13/19 DATE
				SHEET 2 OF 3

RESTORATION PER PAVEMENT CONDITION INDEX (PCI)

THE PAVEMENT CONDITION INDEX, OR PCI, IS A MEASURE OF PAVEMENT CONDITION AND RANGES FROM "0" FOR PAVEMENT THAT IS VERY POOR (OR HAS COMPLETELY FAILED) TO "100" FOR A NEWLY CONSTRUCTED PAVEMENT THAT IS IN VERY GOOD CONDITION.

PAVING CHART

ALL STREET REPAIRS AND PAVEMENT SHALL BE DONE ACCORDING TO PUBLIC WORKS EXCAVATION GUIDELINES AND STANDARD PLANS.

<u>PCI RANGE</u>	<u>CONDITION/CASE</u>	<u>TRENCH RESURFACING</u>
70 - 100	■ VERY GOOD (I)	MORATORIUM.....HALF TO FULL STREET RE-PAVING *
55 - 69	■ GOOD (II/III)	MORATORIUM.....ONE LANE RE-PAVING MIN. *
25 - 54	■ POOR (IV)	T-CUT PER GENERAL NOTE #6 OF SHEET 2
0 - 24	■ VERY POOR (V)	T-CUT PER GENERAL NOTE #6 OF SHEET 2

* AS DETERMINED BY CITY ENGINEER
(PCI RANGE 90-100 STRICTLY ENFORCED)

GENERAL NOTES:

1. MORATORIUM STREETS: 2" GRIND AND OVERLAY WITH IN KIND PAVEMENT OR AS DETERMINED BY THE CITY ENGINEER; IF PAVEMENT RESTORATIONS IS WITHIN 2' OR LESS AT EDGE OF GUTTER OR FLOWLINE, PAVING WILL BE EXTENDED TO THOSE LIMITS.
2. PORTLAND CONCRETE CEMENT (PCC) STREETS WILL REQUIRE FULL PANEL REPLACEMENT IF IMPACTED, DOWELING IS REQUIRED PER CASE IV, SHEET 2 OF 3, IN THIS STANDARD. ANY PCC STREET WITH POOR OR BELOW PCI SHALL BE RESTORED AS DETERMINED BY THE CITY ENGINEER.
3. ANY AC STREETS WITH A PCC SLAB UNDERNEATH, WILL REQUIRE SLAB RESTORATION PER CASE III, OF THIS STANDARD INCLUDING DOWELING.
4. IF ONE TRENCH IS 16 SQUARE FEET OR LESS, MORATORIUM DOES NOT APPLY.
5. MULTIPLE TRENCH CUTS WITHIN 5' OR LESS OF EACH OTHER SHALL BE COMBINED AND RESTORED AS ONE. WHEN SPECIAL FIELD CONDITIONS APPLY, LIMITS OF PAVING COULD BE DETERMINED BY THE CITY ENGINEER.
6. IMPACTED DECORATIVE OR COLORED PAVEMENT, INCLUDING PCC, SHOULD BE REPLACED IN KIND AND PAVING LIMITS SHOULD BE EXTENDED TO THE NEAREST CONCRETE JOINT.
7. ALL STREET RESURFACING/STREET RESTORATION SHALL BE IN A SQUARE OR RECTANGULAR SHAPE.
8. ANY PAVEMENT SCENARIO NOT COVERED IN THIS STANDARD SHOULD BE REPLACED IN KIND OR AS DETERMINED BY THE CITY ENGINEER

CITY OF BURBANK – PUBLIC WORKS DEPARTMENT

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1	RS	12/16	APPROVED BY: 3/13/19	BS-110-3
2	RS	10/17		SHEET 3 OF 3
3	RS/EF	03/01/19		
			CITY ENGINEER	